

## **ADDENDUM #2**

TO: ALL PLANHOLDERS

FROM: CENTRAL ILLINOIS REGIONAL AIRPORT  
3201 CIRA DRIVE  
BLOOMINGTON, ILLINOIS 61704

BY: CRAWFORD, MURPHY & TILLY, INC.

SUBJECT: **ADDENDUM #2**  
TO THE BIDDING DOCUMENTS FOR:  
CONSTRUCT NEW GENERAL AVIATION  
FACILITY – PHASE I  
CENTRAL ILLINOIS REGIONAL AIRPORT  
BLOOMINGTON, ILLINOIS  
AIP PROJECT 3-17-0006-071/077; IL PROJ. BMI-4539  
DATED: June 21, 2022

ADDENDUM  
DATE: June 21, 2022

This Addendum forms part of the bidding and contract documents and is associated with the INVITATION FOR BIDS dated May 10, 2022. This addendum must be acknowledged on Page 21 of Volume 1 of the Contract (Bid) Documents. FAILURE TO DO SO MAY SUBJECT THE BIDDER TO DISQUALIFICATION.

### **VOLUME 1**

1. REVISED BID ADVERTISEMENT  
REPLACE: "June 23" in the first paragraph with "June 30".
2. Division 1 – NOTICE TO BIDDERS / INVITATION FOR BIDS  
REPLACE: "June 23" in the first paragraph with "June 30".
3. Division 2 – BIDDING DOCUMENTS  
REPLACE: "June 23" in Section 22 with "June 30".

### **VOLUME 2**

4. ITEM C-102, SECTION 102-1, Temporary Air and Water Pollution, Soil Erosion, and Siltation Control – Description  
REPLACE: "appropriate location along the haul route" in the fifth paragraph with "location shown in the plans".  
REPLACE: "The Exit shall be designed, maintained, and removed by the Contractor based upon anticipated vehicular/truck traffic over the exit" in the fifth paragraph with "The Exit shall consist of a manufactured temporary unidirectional tracking pad system for the purpose of controlling sediment

track-out from vehicles exiting a construction site. Units previously used in other locations are acceptable, provided they are in good condition and suitable for the use intended. The Exit shall consist of mats arranged together to establish an approximate 24-foot by 28-foot tracking pad system.”

REMOVE: “The Contractor shall reference Illinois Urban Manual Practice 930 in their design, construction, and removal of the Exit” from the sixth paragraph.

5. ITEM C-102, SECTION 102-2, Temporary Air and Water Pollution, Soil Erosion, and Siltation Control – Materials

REPLACE: “**102-2.6 Other**” with “**102-2.7 Other**”.

ADD: “**102-2.6 Stabilized Construction Exit.** The Exit shall consist of one of the following manufactured units:

- (1) FODS Trackout Control Mat System: Manufacturer FODS, 844-200-3637, <http://getfods.com>
- (2) RUBBERFORM Trackout Control Mats: Manufacturer RUBBERFORM, 716-478-0404, [www.trackoutcontrolmats.com](http://www.trackoutcontrolmats.com)
- (3) YAKMAT Outrak Mat: Manufacturer YAKMAT, 866-394-4765, [yakmat.com/access-mats/outrak-mat/](http://yakmat.com/access-mats/outrak-mat/)

6. ITEM C-102, SECTION 102-1, Temporary Air and Water Pollution, Soil Erosion, and Siltation Control – Construction Requirements

ADD: “**102-3.5 Installation, maintenance and removal of stabilized construction exit.** Prior to construction, submittals consisting of the manufacturer’s Technical Data Sheet, Installation Guide, and Product Maintenance Guide as well as layout data showing size and arrangement of mats making up the tracking pad shall be provided. Grade soil where tracking pad will be placed to ensure that installation site is flat, without unneeded high or low spots. If necessary, remove unsuitable material and replace with material capable of supporting construction loads. Compact as required. Install tracking pad mats in an approximate 24-foot by 28-foot footprint, with the 28-foot dimension in the direction of construction traffic travel. Adjacent panels shall be butted together and be placed at approximately the same elevation. Anchor panels against sliding according to manufacturer’s recommendations. Connect mats together per manufacturer’s instructions. Clean dirt and debris from tracking pad as often as necessary or once debris has reached 2.5” in the lane of travel. Properly dispose of material removed so it does not leave the site or enter ditches of stormwater pipes. Once the site is stabilized, or when the project moves into a new phase such that the tracking pad is no longer required, remove tracking pad, taking care to properly dispose of dirt and debris, and restore the area as described in Part 12 TURFING.”

## **PLANS**

7. Plan Sheet GI002 INDEX TO SHEETS

REPLACE: The entirety of this sheet with the revised sheet included in this addendum. Sheet has been revised to include new sheets that have been added to the set as well as altering the names of some of the CONSTRUCTION ACTIVITY PLAN sheets.

8. Plan Sheet GI100 AIRPORT SITE PLAN

REPLACE: The entirety of this sheet with the revised sheet included in this addendum. Sheet has been revised to clarify locations of contractor staging, material & equipment storage, and engineer’s field office.

9. Plan Sheet GC002 CONSTRUCTION ACTIVITY NOTES 2

REPLACE: The entirety of this sheet with the revised sheet included in this addendum. Sheet has been revised to swap the notes between Sections 19 and 20, in order to be more logical. Also, the note #5 previously under Section 20 related to the use of temporary gates has been removed since it is not applicable.

10. Plan Sheet GC003 CONSTRUCTION ACTIVITY DETAILS

REPLACE: The entirety of this sheet with the revised sheet included in this addendum. A note has been added to Detail #2 referencing FAA AC 150/5345-55A, and Detail #6, BARRIER FENCE, has been added.

11. Plan Sheets GC101 through GC103 CONSTRUCTION ACTIVITY PLANS

REPLACE: The entirety of these three sheets with the revised sheets included in this addendum. In general, all sheets have been revised so that RWY 11/29 will predominately be open during construction activities, not closed. Sheet GC101 (Phase 1A) specifies how construction shall be conducted when the RWY is open. Sheet GC102 (Phase 1B) specifies how construction shall be conducted within the Runway Safety Area when the RWY is closed (for a duration not exceeding 60 consecutive calendar days). And Sheet GC103 (Phase 1C) provides for periodic operation within the Runway Safety Area (10 non-consecutive days) where the RWY shall be closed after 7:00 AM and reopened by 7:00 PM.

12. Plan Sheet LG101 EROSION CONTROL & TURFING PLANS 1

REVISE: The leader for "PROPOSED STABILIZED CONSTRUCTION EXIT (C-102)" currently points to the intersection of FUT. TXY B1 and RWY 11/29. The location for this exit should be relocated approximately 350 feet to the east to the location of RWY 11/ 29 and the existing Taxiway that is scheduled for demolition.

**ATTACHMENTS**

- Updated Plan Sheet GI002 INDEX TO SHEETS
- Updated Plan Sheet GI100 AIRPORT SITE PLAN
- Updated Plan Sheet GC002 CONSTRUCTION ACTIVITY NOTES 2
- Updated Plan Sheet GC003 CONSTRUCTION ACTIVITY DETAILS
- Updated Plan Sheet GC101 CONSTRUCTION ACTIVITY PLAN PHASE 1-A
- Updated Plan Sheet GC102 CONSTRUCTION ACTIVITY PLAN PHASE 1-B
- Updated Plan Sheet GC103 CONSTRUCTION ACTIVITY PLAN PHASE 1-C

**ACKNOWLEDGEMENT**

**OF ADDENDUM #2:**

To verify that all Contractors are in receipt of this addendum, Contractors are asked to sign and date this acknowledgement sheet. The Contractor should email [msewell@cmtengr.com](mailto:msewell@cmtengr.com) by June 28, 2022. This addendum must also be acknowledged on Page 21 of Volume 1 of the Contract (Bid) Documents. FAILURE TO DO SO MAY SUBJECT THE BIDDER TO DISQUALIFICATION.

To: Crawford, Murphy & Tilly, Inc.  
Attention: Michael Sewell – [msewell@cmtengr.com](mailto:msewell@cmtengr.com)

The undersigned acknowledges receipt of Addendum #2:

Date Received: \_\_\_\_\_

Received By: \_\_\_\_\_

Representing: \_\_\_\_\_

City & State: \_\_\_\_\_



License No. 184-000613

CONSULTANTS

Sheet List Table		
Sheet No.	Sheet Index	Sheet Title
1	GI001	COVER SHEET
2	GI002	INDEX TO SHEETS
3	GI003	SUMMARY OF QUANTITIES
4	GI004	EXISTING CONDITIONS LEGEND
5	GI100	AIRPORT SITE PLAN
6	GI200	ALIGNMENT INFORMATION
7	GC001	CONSTRUCTION ACTIVITY NOTES 1
8	GC002	CONSTRUCTION ACTIVITY NOTES 2
9	GC003	CONSTRUCTION ACTIVITY DETAILS
10	GC101	CONSTRUCTION ACTIVITY PLAN PHASE 1-A
11	GC102	CONSTRUCTION ACTIVITY PLAN PHASE 1-B
12	GC103	CONSTRUCTION ACTIVITY PLAN PHASE 1-C
13	CD001	EXISTING PAVEMENT REMOVAL PLAN
14	CD101	EXISTING CONDITIONS & REMOVALS 1
15	CD102	EXISTING CONDITIONS & REMOVALS 2
16	CD103	EXISTING CONDITIONS & REMOVALS 3
17	CD104	EXISTING CONDITIONS & REMOVALS 4
18	CP100	PROPOSED MILLINGS ROAD
19	CP101	PROPOSED GEOMETRY 1
20	CP102	PROPOSED GEOMETRY 2
21	CP201	PLAN & PROFILE - TXY B
22	CP202	PLAN & PROFILE - TXY B1 & TXY B2
23	CP203	PLAN & PROFILE - TXY H1 & TXY H2
24	CP204	PLAN & PROFILE - INTERIOR TAXILANES
25	CP301	TYPICAL SECTIONS 1
26	CP302	TYPICAL SECTIONS 2
27	CG101	GRADING & DRAINAGE PLAN 1
28	CG102	GRADING & DRAINAGE PLAN 2
29	CG103	UNDERDRAIN PLAN
30	CG201	STORM SEWER PROFILE 1
31	CG202	STORM SEWER PROFILE 2
32	CG203	STORM SEWER PROFILE 3
33	CG204	STORM SEWER PROFILE 4
34	CG205	STORM SEWER PROFILE 5
35	CU501	UNDERDRAIN DETAILS
36	CU502	3' INLET DETAIL
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47	EL102	ELECTRICAL PLAN 2
48	EL501	ELECTRICAL DETAIL 1
49	EL502	ELECTRICAL DETAIL 2
50	EL503	ELECTRICAL DETAIL 3
51	EL504	ELECTRICAL DETAIL 4
52	CG600	CROSS SECTION INDEX
53	CG601	SUMMARY OF EARTHWORK QUANTITIES
54	CG601	TXY B SECTION 1
55	CG602	TXY B SECTION 2
56	CG603	TXY B SECTION 3
57	CG604	TXY B SECTION 4
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61	CG608	TXY B SECTION 8
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63	CG610	TXY B1 CROSS SECTIONS 1
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68	CG615	TAXIWAY H1 CROSS SECTION 3
69	CG616	TAXIWAY H1 CROSS SECTION 4
70	CG617	TAXIWAY H1 CROSS SECTION 5
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72	CG619	TAXIWAY H2 CROSS SECTION 2
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75	CG622	TAXIWAY H2 CROSS SECTION 5
76	CG623	TAXIWAY H2 CROSS SECTION 6
77	CG624	VEH. PARKING SECTION 1
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100% FOR BIDDING  
MAY 23, 2022

CONSTRUCT NEW GENERAL  
AVIATION FACILITY - PHASE I

OWNER



BLOOMINGTON-NORMAL  
AIRPORT AUTHORITY  
CENTRAL ILLINOIS REGIONAL  
AIRPORT  
BLOOMINGTON, ILLINOIS

△	6/15/22	ADDENDUM #1
△	6/21/22	ADDENDUM #2

MARK	DATE	DESCRIPTION

AIP PROJ. NO. 3-17-0006-071/077

IL PROJ. NO. BMI-4539

CMT PROJECT NO: 19008501-10

CAD DWG FILE: 19008501-PH1-GI002.DWG

DESIGNED BY: EMH

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: MBS

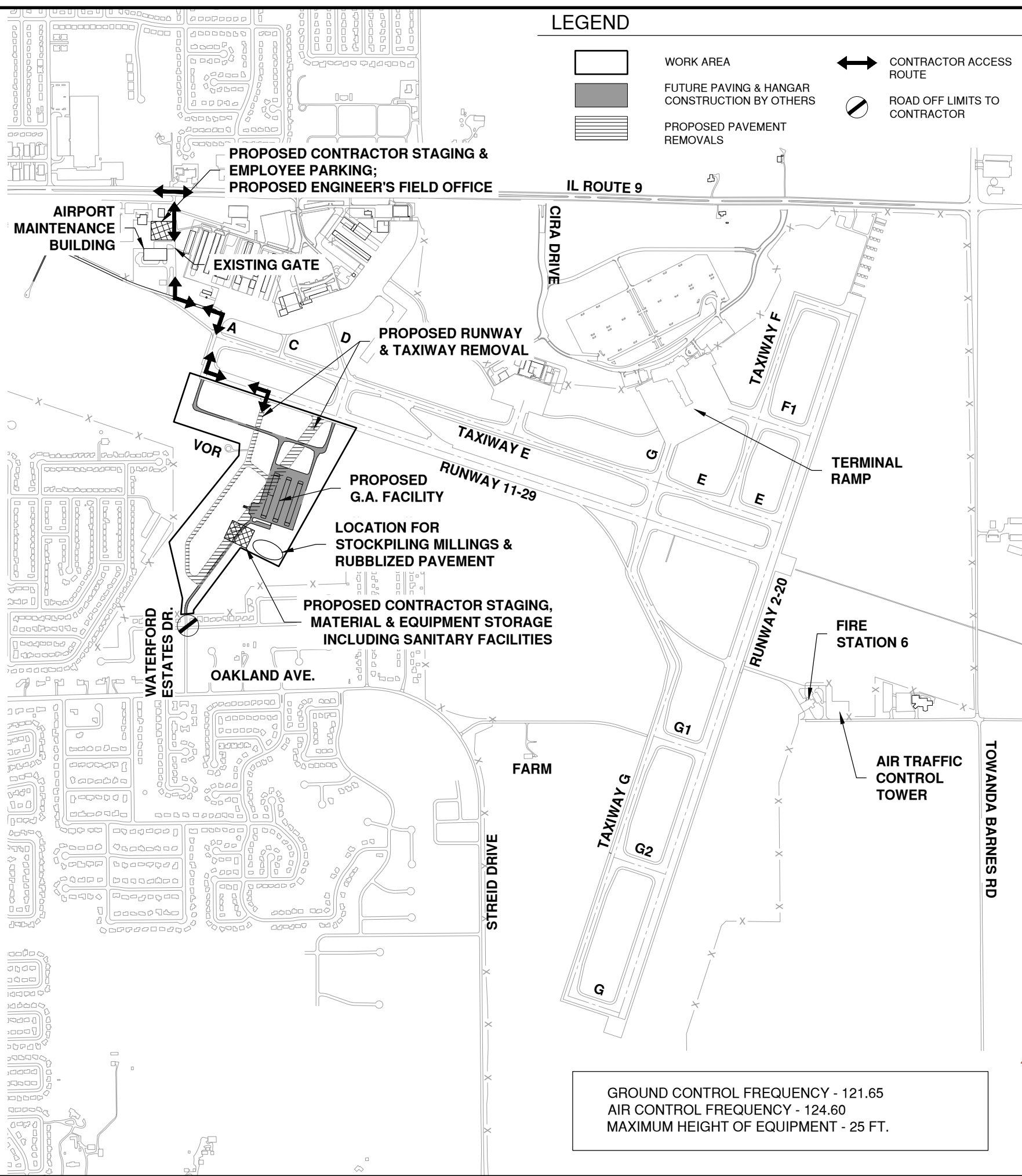
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SHEET TITLE

INDEX TO SHEETS

GI002  
SHEET 2 OF 88

Path: K:\Bloomington\19008501\_GA\_Facility\Draw\_Ph1\Sheets Phase 1\19008501-PH1-G100.dwg  
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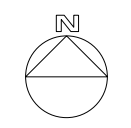


**LEGEND**

-  WORK AREA
-  FUTURE PAVING & HANGAR CONSTRUCTION BY OTHERS
-  PROPOSED PAVEMENT REMOVALS
-  CONTRACTOR ACCESS ROUTE
-  ROAD OFF LIMITS TO CONTRACTOR



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CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100% FOR BIDDING  
MAY 23, 2022

CONSTRUCT NEW GENERAL AVIATION FACILITY - PHASE I

OWNER



BLOOMINGTON-NORMAL AIRPORT AUTHORITY  
CENTRAL ILLINOIS REGIONAL AIRPORT  
BLOOMINGTON, ILLINOIS

6/21/22 ADDENDUM #2

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AIP PROJ. NO. 3-17-0006-071/077		
IL PROJ. NO. BMI-4539		
CMT PROJECT NO: 19008501-10		
CAD DWG FILE: 19008501-PH1-G100.DWG		
DESIGNED BY: EMH		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: MBS		
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**AIRPORT SITE PLAN**

GROUND CONTROL FREQUENCY - 121.65  
 AIR CONTROL FREQUENCY - 124.60  
 MAXIMUM HEIGHT OF EQUIPMENT - 25 FT.

**16. HAZARD MARKING & LIGHTING**

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D (OR LATEST) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN IN THE PLANS OR AS DIRECTED BY THE AIRPORT.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE LIGHTS AND FLAG PLACEMENT.

**17. LIGHTING FOR NIGHTTIME CONSTRUCTION**

1. THE CONTRACTOR SHALL PROVIDE ADEQUATE LIGHTING DURING NIGHTTIME CONSTRUCTION.
2. ARTIFICIAL AREA LIGHTING SHALL CONSIST OF VEHICLE OR POLE MOUNTED FLOODLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL ONLY BE ALLOWED IN ADDITION TO THE AREA LIGHTING.
3. ARTIFICIAL AREA LIGHTING SHALL NOT INTERFERE WITH AIR TRAFFIC OR ATCT OPERATIONS.
4. PLACEMENT & AIMING OF ARTIFICIAL LIGHTING SHALL BE APPROVED BY THE AIRPORT PRIOR TO START OF OPERATIONS.

**18. PROTECTION OF AREAS & SURFACES**

1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 2-20 OR 11-29 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 130' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
3. ALL WORK REQUIRED ON AN ACTIVE TAXILANE OBJECT FREE AREA, WHICH EXTENDS 113' FROM THE TAXILANE/APRON CENTERLINE, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

**19. OTHER LIMITATIONS OF CONSTRUCTION**

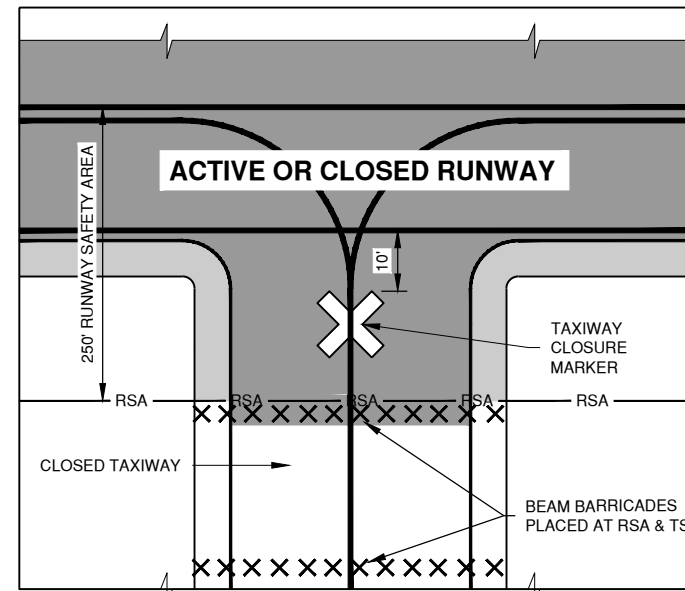
1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE SPECIFICATIONS.

**20. AIRPORT SECURITY REQUIREMENTS**

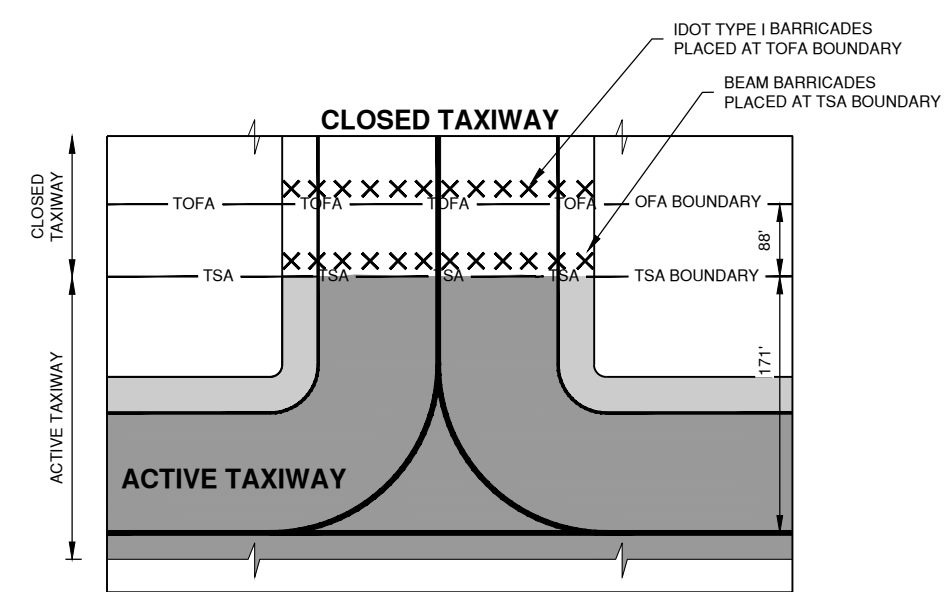
1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR. THE SUPERINTENDENT AND FOREMAN THAT ARE ISSUED BADGES SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD. BADGES SHALL BE RETURNED TO THE AIRPORT AT THE FINAL INSPECTION OR WHEN THE PERSON IS NO LONGER EMPLOYED BY THE CONTRACTOR. THE CONTRACTOR WILL PAY A FEE OF \$200.00 WITHIN 15 DAYS FOR EACH ACCESS BADGE THAT IS LOST, DESTROYED, STOLEN, OR NOT RETURNED AT THE FINAL INSPECTION.

**20. AIRPORT SECURITY REQUIREMENTS (CONT'D)**

3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES.
4. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
6. AS A MINIMUM, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURITY DURING CONSTRUCTION AS FOLLOWS:
  - a. POSSESS A COPY OF THE AIRPORT'S PROJECT SECURITY PLAN.
  - b. VISIBLY DELINEATE HIS CONSTRUCTION ZONE BY PLACING A LINE OF BARRICADES OR FLAGGING AROUND THE ENTIRE WORK ZONE DURING EACH PHASE OF THE CONTRACT.
  - c. COMPLY WITH THE AIRPORT'S SECURITY PLAN ASSOCIATED WITH THE CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SECURITY PROCEDURES AND REGULATIONS ON THE AIRPORT.
  - d. ENSURE THAT NO CONSTRUCTION EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE AIRCRAFT OPERATIONS AREA FROM CONSTRUCTION SITE UNLESS AUTHORIZED.
  - e. THE AIRPORT MAY REQUIRE THAT ALL SECURITY GUARDS UNDERGO ADDITIONAL TRAINING NECESSARY TO MEET THE AIRPORT'S SECURITY NEEDS.
  - f. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN SECURITY ON THE AIRPORT AS SPECIFIED OR AS DIRECTED BY THE AIRPORT.
  - g. THE CONTRACTOR'S SUPERINTENDENT, FOREMAN, SECURITY GUARDS, AND ANY SUPERVISORY PERSONNEL IN CHARGE OF OTHER WORKERS SHALL OBTAIN AN AIRPORT AUTHORITY SECURITY BADGE AND DISPLAY THIS BADGE WHILE ON SITE IN ACCORDANCE WITH FAA AND TSA REGULATIONS. CONTRACTOR PERSONNEL WITH BADGES SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD.
  - h. TO OBTAIN AIRPORT SECURITY BADGES, CONTRACTORS MUST COMPLETE A CRIMINAL HISTORY RECORDS CHECK TWO WEEKS PRIOR TO EMPLOYEES BEING ALLOWED ACCESS TO THE SITE. THE TWO-WEEK PERIOD IS NECESSARY FOR AN ADEQUATE TIME OF PROCESSING FINGERPRINTS FOR COMPLETING THE CRIMINAL HISTORY CHECK. THE CONTRACTOR IS REQUIRED TO DEPOSIT A \$200 FEE TO THE AIRPORT PER BADGE, WHICH IS 80% REFUNDABLE AFTER EACH BADGE IS RETURNED. THE AIRPORT MUST RECEIVE THIS FEE/DEPOSIT PRIOR TO CONDUCTING ANY OF THE SECURITY BADGE ISSUE PROCESS. THE CONTRACTOR IS REQUIRED TO CONTACT THE AIRPORT AT LEAST THREE CALENDAR DAYS PRIOR TO SCHEDULING FINGERPRINTING AND BADGE TRAINING. TRAINING LASTS APPROXIMATELY TWO HOURS AND CAN BE CONDUCTED INDIVIDUALLY OR WITH A GROUP.
  - i. THE CONTRACTOR WILL DESIGNATE A MINIMUM OF ONE INDIVIDUAL TO BE THE 24-HOUR POINT OF CONTACT AND ASSUME ALL ON-SITE SECURITY RESPONSIBILITIES FOR ALL EMPLOYEES DURING THE PROJECT. THIS INDIVIDUAL SHALL PROVIDE THE AIRPORT A 24-HOUR CONTACT FOR EMERGENCY PURPOSES. THIS INDIVIDUAL WILL ALSO BE REQUIRED TO HAVE AVAILABLE AND PRESENT UPON REQUEST AT ANY TIME; A COPY OF THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) APPROVED CHANGE OF CONDITION DURING THE PROJECT.



**1** CLOSED CONNECTING TAXIWAY DETAIL  
N.T.S.



**4** CLOSED TAXIWAY TO ACTIVE TAXIWAY DETAIL  
N.T.S.



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100% FOR BIDDING  
MAY 23, 2022

CONSTRUCT NEW GENERAL  
AVIATION FACILITY - PHASE I



BLOOMINGTON-NORMAL  
AIRPORT AUTHORITY  
CENTRAL ILLINOIS REGIONAL  
AIRPORT  
BLOOMINGTON, ILLINOIS

6/21/22 ADDENDUM #2

MARK DATE DESCRIPTION

AIP PROJ. NO. 3-17-0006-071/077

IL PROJ. NO. BMI-4539

CMT PROJECT NO: 19008501-10

CAD DWG FILE: 19008501-PH1-GC000.DWG

DESIGNED BY: EMH

DRAWN BY: DPA

CHECKED BY: MJD

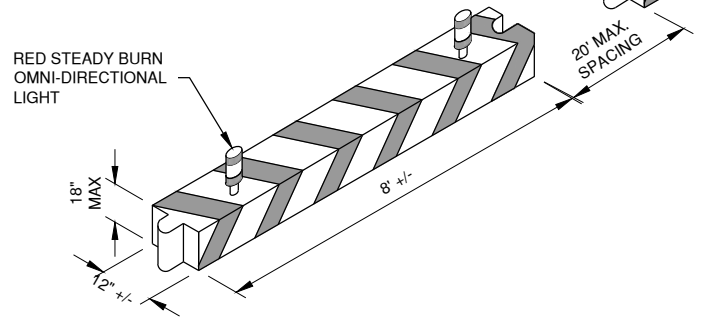
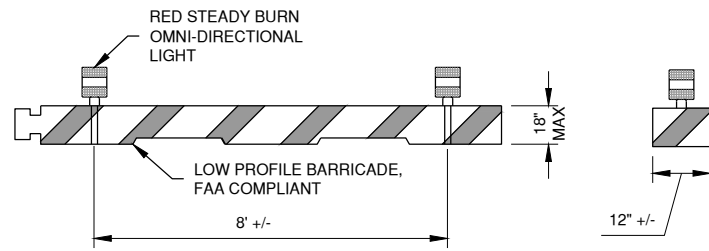
APPROVED BY: MBS

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SHEET TITLE  
**CONSTRUCTION  
ACTIVITY NOTES 2**

GC002  
SHEET 8 OF 88

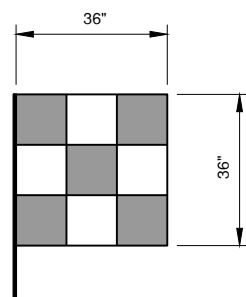
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Date: Tuesday, June 21, 2022 2:24:54 PM



**1** BEAM BARRICADE DETAIL  
N.T.S.

**BEAM BARRICADE NOTES**

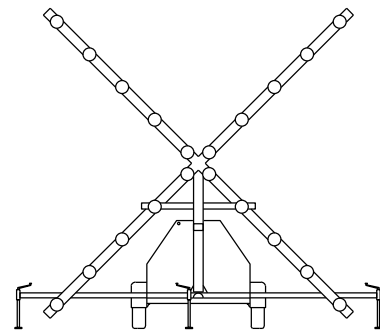
- BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
- BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE ENGINEER.
- BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).



**4** EQUIPMENT & VEHICLE SIGNAL FLAG  
N.T.S. (ORANGE / WHITE)

**SIGNAL FLAGNOTES**

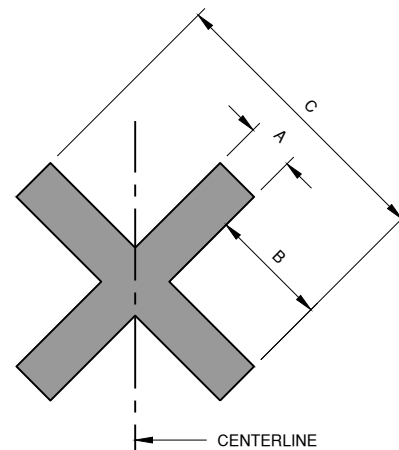
- ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
- WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
- CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.



**2** LIGHTED RUNWAY CLOSURE MARKER  
N.T.S.

**LIGHTED RUNWAY CLOSURE MARKER NOTES**

- LIGHTED RUNWAY CLOSURE EQUIPMENT REQUIREMENTS SHALL BE COMPLIANT WITH FAA AC 150/5345-55A.
- TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS DURING RUNWAY CLOSURE.
- THE CONTRACTOR SHALL PROVIDE TWO CLOSURE MARKERS (2 PAIR) AND MAINTAIN THEM (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS ASSOCIATED WITH PROVIDING AND MAINTAINING THIS ITEM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

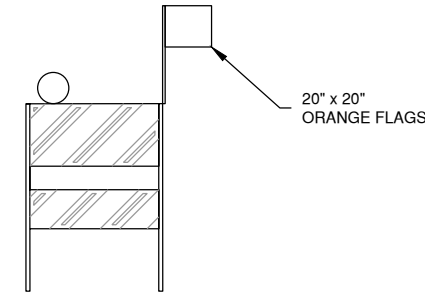


DIMENSION	A	B	C
CLOSED TAXIWAY	5'-0"	12'-6"	30'-0"
CLOSED RUNWAY	10'-0"	25'-0"	60'-0"

**5** NON-LIGHTED CLOSURE MARKER  
N.T.S.

**NON-CLOSURE MARKER NOTES**

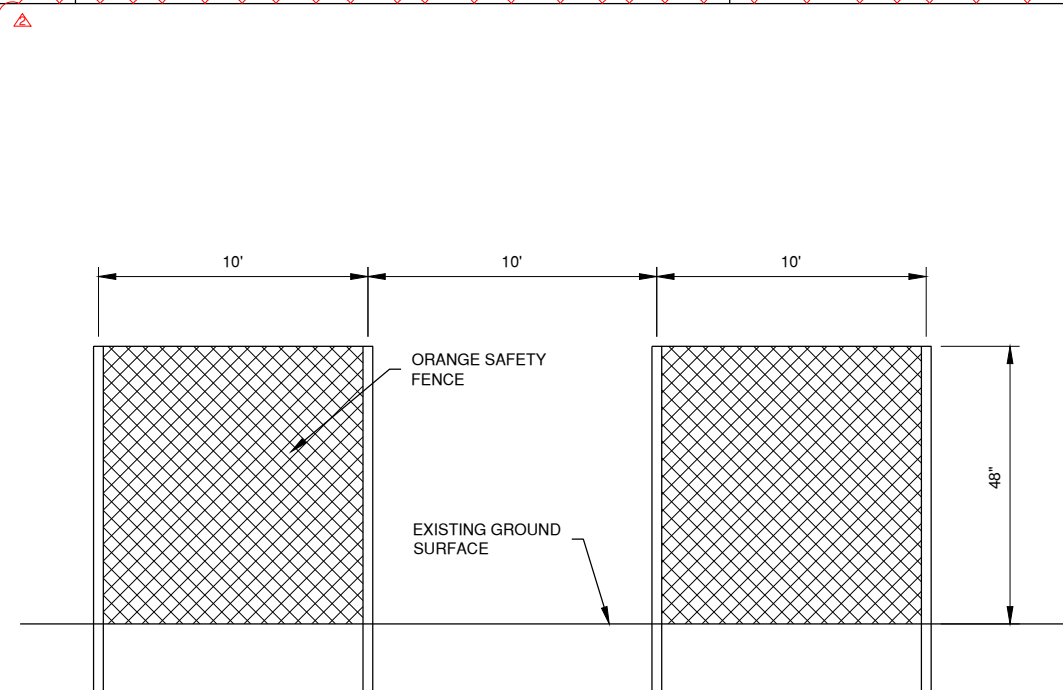
- CLOSURE MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
- MARKERS SHALL BE PLACED ON RUNWAYS TO COVER THE NUMERALS ON BOTH ENDS.
- MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.



**3** FLASHER BARRICADE - IDOT TYPE I  
N.T.S.

**FLASHER BARRICADE NOTES**

- FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AT 20' INTERVALS.



**6** BARRIER FENCE  
N.T.S.

**BARRIER FENCE NOTES**

- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AT 10' INTERVALS ALONG RUNWAY 11-29 SAFETY AREA (RSA).
- PLACE AND MAINTAIN UNTIL REMOVAL ACCORDING TO CSPP.

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MAY 23, 2022

CONSTRUCT NEW GENERAL  
AVIATION FACILITY - PHASE I

OWNER



BLOOMINGTON-NORMAL  
AIRPORT AUTHORITY  
CENTRAL ILLINOIS REGIONAL  
AIRPORT  
BLOOMINGTON, ILLINOIS

6/21/22 ADDENDUM #2

MARK DATE DESCRIPTION

AIP PROJ. NO. 3-17-0006-071/077

IL PROJ. NO. BMI-4539

CMT PROJECT NO: 19008501-10

CAD DWG FILE: 19008501-PH1-GC000.DWG

DESIGNED BY: EMH

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: MBS

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SHEET TITLE

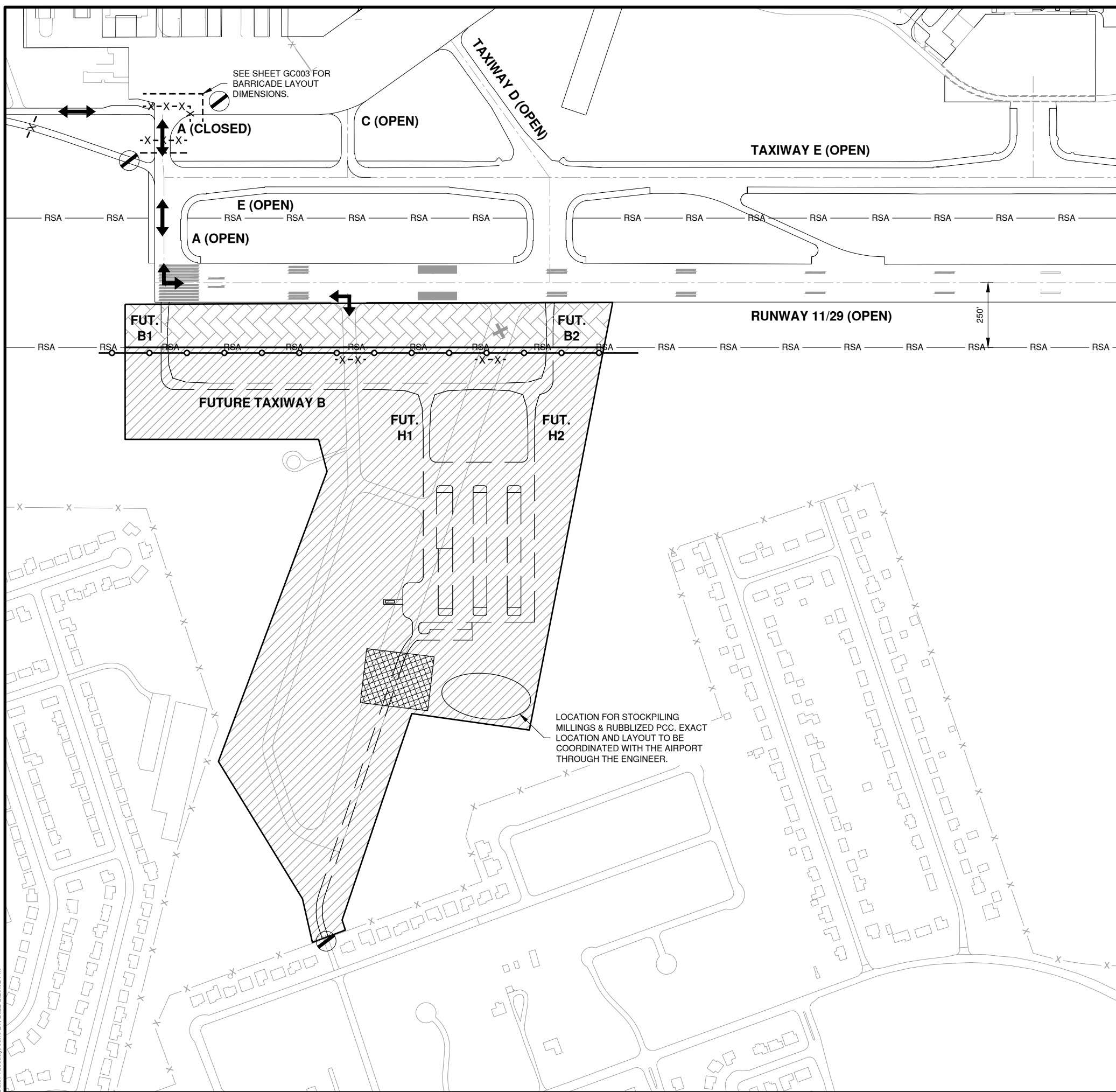
CONSTRUCTION  
ACTIVITY DETAILS

GC003

SHEET 9 OF 88



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**PHASE 1A NOTES**

1. RUNWAY 11/29 WILL REMAIN OPEN FOR THIS PHASE.
2. ALL EQUIPMENT SHALL BE MOVED TO THE STAGING AREA AT THE END OF EACH WORK PERIOD.
3. FOR THIS PHASE, THE CONTRACTOR SHALL PROVIDE A FULL TIME ESCORT IN CONSTANT RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) TO ESCORT HIS EMPLOYEES, SUBCONTRACTORS, AND DELIVERIES TO AND FROM THE AVAILABLE WORK AREA. ALL ACCESS TO AND FROM THE AVAILABLE WORK AREAS SHALL BE UNDER THE CONTROL OF THE CONTRACTOR'S ESCORT.
4. INGRESS AND EGRESS TO THE PROPOSED WORK AREAS WILL REQUIRE ACCESS TO AND CROSSING OF PAVEMENTS UNDER ACTIVE AIR TRAFFIC CONTROL TOWER (ATCT) CONTROL. TO MINIMIZE IMPACTS TO AIR TRAFFIC AND THE ATCT WORKLOAD, THE CONTRACTOR SHALL COORDINATE AND PLAN THEIR ACCESS TO AND FROM THE SITE TO OCCUR WITHIN THE MINIMUM NUMBER OF TRIPS AS POSSIBLE.
5. THE CONTRACTOR SHALL PROVIDE 72 HOUR NOTICE PRIOR TO INITIATING THIS PHASE.
6. THE CONTRACTOR SHALL PLACE ALL RUNWAY CLOSURE MARKERS, TAXIWAY CLOSURE MARKERS, AND BARRICADES PRIOR TO STARTING WORK ON THIS PHASE.
7. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN ACTIVE PAVEMENTS DURING HAULING OPERATIONS AND AT THE END OF THE WORKDAY.
8. AIRCRAFT AND AIRPORT VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
9. THIS PHASE WILL RELY HEAVILY ON COORDINATION WITH THE AIR TRAFFIC CONTROL TOWER ( ATCT) THROUGH RADIO COMMUNICATIONS TO COMPLETE THIS WORK. THE CONTRACTOR WILL NOT BE ABLE TO ACCESS THIS WORK AREA WITHOUT BEING UNDER THE NECESSARY SECURITY AND RADIO CONTROL.

**PAVEMENT STATUS**

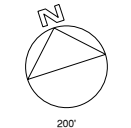
PAVEMENT	PHASE 1A
RUNWAY 11-29	OPEN
TAXIWAY A	CLOSED BETWEEN T/W E AND APRON
TAXIWAY C	OPEN
TAXIWAY D	OPEN
TAXIWAY E	OPEN

**LEGEND**

- AVAILABLE WORK AREA FOR THIS PHASE
- CONTRACTOR STAGING & STORAGE AREA
- AREA NOT AVAILABLE FOR WORK IN THIS PHASE
- OFF LIMITS TO CONTRACTOR ACCESS
- CONTRACTOR ACCESS ROUTE
- BEAM BARRICADES
- IDOT TYPE I BARRICADES
- BARRIER FENCE



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MAY 23, 2022

CONSTRUCT NEW GENERAL AVIATION FACILITY - PHASE I

OWNER



BLOOMINGTON-NORMAL AIRPORT AUTHORITY  
CENTRAL ILLINOIS REGIONAL AIRPORT  
BLOOMINGTON, ILLINOIS

MARK	DATE	DESCRIPTION
▲	6/21/22	ADDENDUM #2
AIP PROJ. NO. 3-17-0006-071/077		
IL. PROJ. NO. BMI-4539		
CMT PROJECT NO: 19008501-10		
CAD DWG FILE: 19008501-PH1-GC100.DWG		
DESIGNED BY: EMH		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: MBS		
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**CONSTRUCTION ACTIVITY PLAN PHASE 1-A**

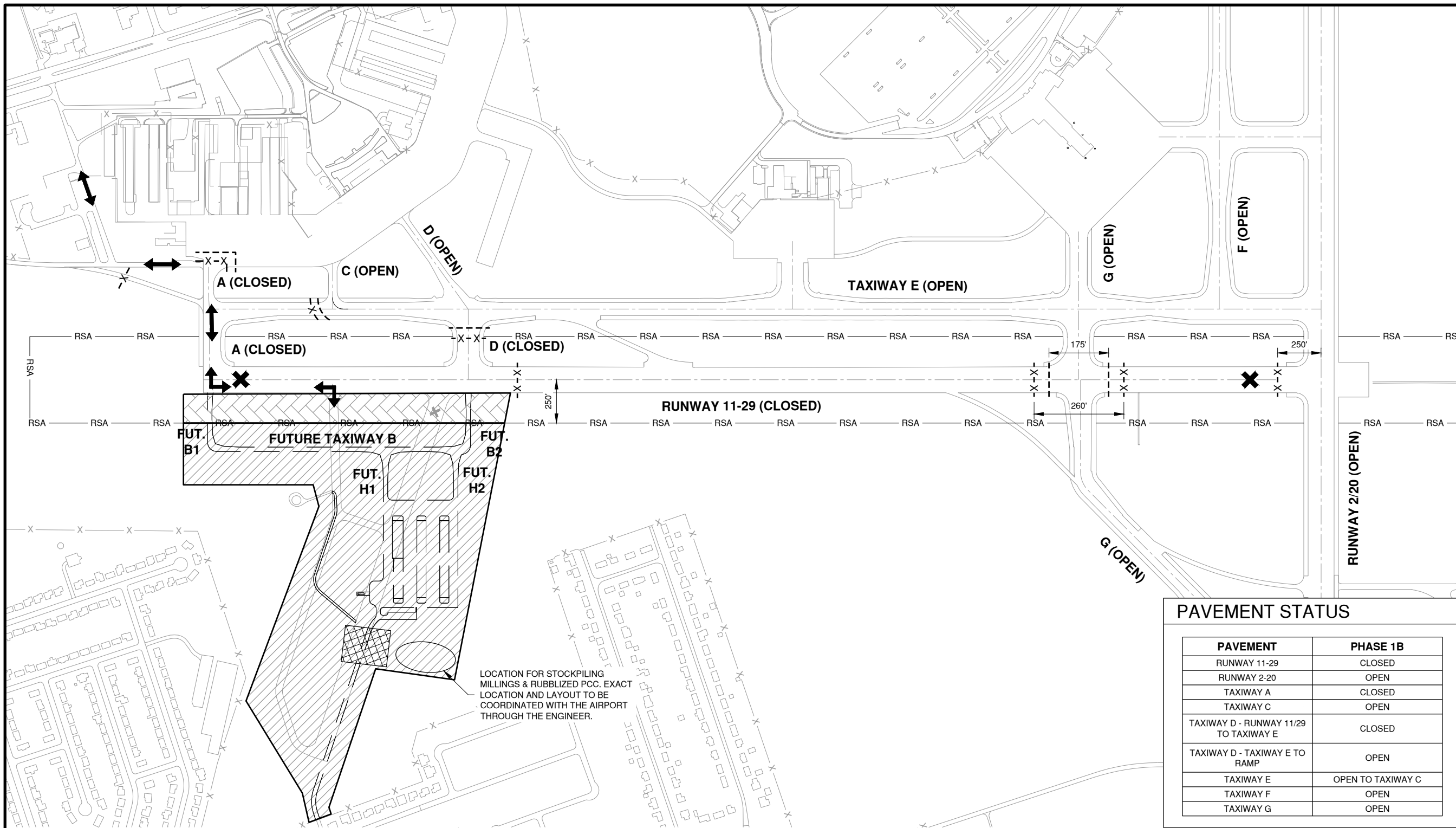


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0 100' 200'  
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MAY 23, 2022

CONSTRUCT NEW GENERAL  
AVIATION FACILITY - PHASE I

OWNER



BLOOMINGTON-NORMAL  
AIRPORT AUTHORITY  
CENTRAL ILLINOIS REGIONAL  
AIRPORT  
BLOOMINGTON, ILLINOIS

PAVEMENT STATUS

PAVEMENT	PHASE 1B
RUNWAY 11-29	CLOSED
RUNWAY 2-20	OPEN
TAXIWAY A	CLOSED
TAXIWAY C	OPEN
TAXIWAY D - RUNWAY 11/29 TO TAXIWAY E	CLOSED
TAXIWAY D - TAXIWAY E TO RAMP	OPEN
TAXIWAY E	OPEN TO TAXIWAY C
TAXIWAY F	OPEN
TAXIWAY G	OPEN

LOCATION FOR STOCKPILING MILLINGS & RUBBLIZED PCC. EXACT LOCATION AND LAYOUT TO BE COORDINATED WITH THE AIRPORT THROUGH THE ENGINEER.

LEGEND

- AVAILABLE WORK AREA FOR THIS PHASE
- CONTRACTOR STAGING & STORAGE AREA
- EXPEDITED WORK AREA FOR THIS PHASE
- IDOT TYPE I BARRICADES
- BEAM BARRICADES
- RUNWAY CLOSURE MARKER (LIGHTED)
- CONTRACTOR ACCESS ROUTE

PHASE 1B NOTES

1. RUNWAY 11/29 WILL BE CLOSED FOR THIS PHASE.
2. ALL EQUIPMENT SHALL BE MOVED TO THE STAGING AREA AT THE END OF EACH WORK PERIOD.
3. THE CONTRACTOR SHALL PROVIDE 7 CALENDAR DAYS NOTICE PRIOR TO INITIATING THIS PHASE.
4. THE CONTRACTOR SHALL PLACE ALL RUNWAY CLOSURE MARKERS AND BARRICADES PRIOR TO STARTING WORK ON THIS PHASE.
5. THIS PHASE MAY BE CONCURRENT WITH PHASE 1-A.
6. WORK WITHIN 250' OF THE RUNWAY 11/29 CENTERLINE IS AN EXPEDITED WORK AREA AND SHALL RECEIVE PRIORITY OVER ALL OTHER WORK AREAS.
7. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN THE ACCESS ROUTE AND AT THE END OF THE WORKDAY.
8. AIRCRAFT AND AIRPORT VEHICLES HAVE THE RIGHT OF WAY AT ALL TIMES.
9. THE IMPACTS OF THIS PHASE MERIT ADDITIONAL CONSIDERATION FROM THE CONTRACTOR WHEN SCHEDULING THIS WORK. AS A RESULT, THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE SUFFICIENT PERSONNEL AND EQUIPMENT AND WORK SUFFICIENT HOURS TO COMPLETE THIS PHASE WITHIN THE REQUIRED PARAMETERS.
10. THE CONTRACTOR WILL BE ALLOWED TO CLOSE RUNWAY 11/29 FOR NO MORE THAN 60 CONSECUTIVE CALENDAR DAYS TO PERFORM THE WORK WITHIN THIS EXPEDITED WORK AREA.
11. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT TO SCHEDULE THE CLOSURE. VARIABLES SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT AVAILABILITY & CONDITION, AND WORKFORCE AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE.
12. PRIOR TO RE-OPENING RUNWAY 11/29, THE CONTRACTOR SHALL MEET THE FOLLOWING REQUIREMENTS WITHIN 250' OF RUNWAY 11/29 CENTERLINE:
  - 12.1. CLEARED AND GRADED AND HAVE NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS.
  - 12.2. DRAINED BY GRADING OR STORM SEWERS TO PREVENT WATER ACCUMULATION.
  - 12.3. CAPABLE, UNDER DRY CONDITIONS, OF SUPPORTING AIRPORT MAINTENANCE EQUIPMENT, AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) EQUIPMENT, AND THE OCCASIONAL PASSAGE OF AIRCRAFT WITHOUT CAUSING DAMAGE TO THE AIRCRAFT.
  - 12.4. FREE OF OBJECTS HIGHER THAN 3" ABOVE GRADE INCLUDING SOIL MOUNDS, SURFACE IRREGULARITIES, OR CONSTRUCTION MATERIALS.
  - 12.5. OPEN TRENCHES OR EXCAVATIONS MUST BE BACKFILLED.
13. FOR THIS PHASE, THE CONTRACTOR WILL BE REQUIRED TO DISABLE RUNWAY 11/29 EDGE LIGHTS IN THE VAULT.
14. FOR THIS PHASE, THE CONTRACTOR SHALL PROVIDE A FULL TIME ESCORT IN CONSTANT RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) TO ESCORT HIS EMPLOYEES, SUBCONTRACTORS, AND DELIVERIES TO AND FROM THE AVAILABLE WORK AREA. ALL ACCESS TO AND FROM THE AVAILABLE WORK AREAS SHALL BE UNDER THE CONTROL OF THE CONTRACTOR'S ESCORT.
15. INGRESS AND EGRESS TO THE PROPOSED WORK AREAS WILL REQUIRE ACCESS TO AND CROSSING OF PAVEMENTS UNDER ACTIVE AIR TRAFFIC CONTROL TOWER (ATCT) CONTROL. TO MINIMIZE IMPACTS TO AIR TRAFFIC AND THE ATCT WORKLOAD, THE CONTRACTOR SHALL COORDINATE AND PLAN THEIR ACCESS TO AND FROM THE SITE TO OCCUR WITHIN THE MINIMUM NUMBER OF TRIPS AS POSSIBLE.
16. THIS PHASE WILL RELY HEAVILY ON COORDINATION WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) THROUGH RADIO COMMUNICATIONS TO COMPLETE THIS WORK. THE CONTRACTOR WILL NOT BE ABLE TO ACCESS THIS WORK AREA WITHOUT BEING UNDER THE NECESSARY SECURITY AND RADIO CONTROL.

MARK DATE DESCRIPTION

AIP PROJ. NO. 3-17-0006-071/077

IL PROJ. NO. BMI-4539

CMT PROJECT NO: 19008501-10

CAD DWG FILE: 19008501-PH1-GC100.DWG

DESIGNED BY: EMH

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: MBS

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SHEET TITLE  
**CONSTRUCTION  
ACTIVITY PLAN PHASE  
1-B**

GC102

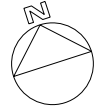
SHEET 11 OF 88

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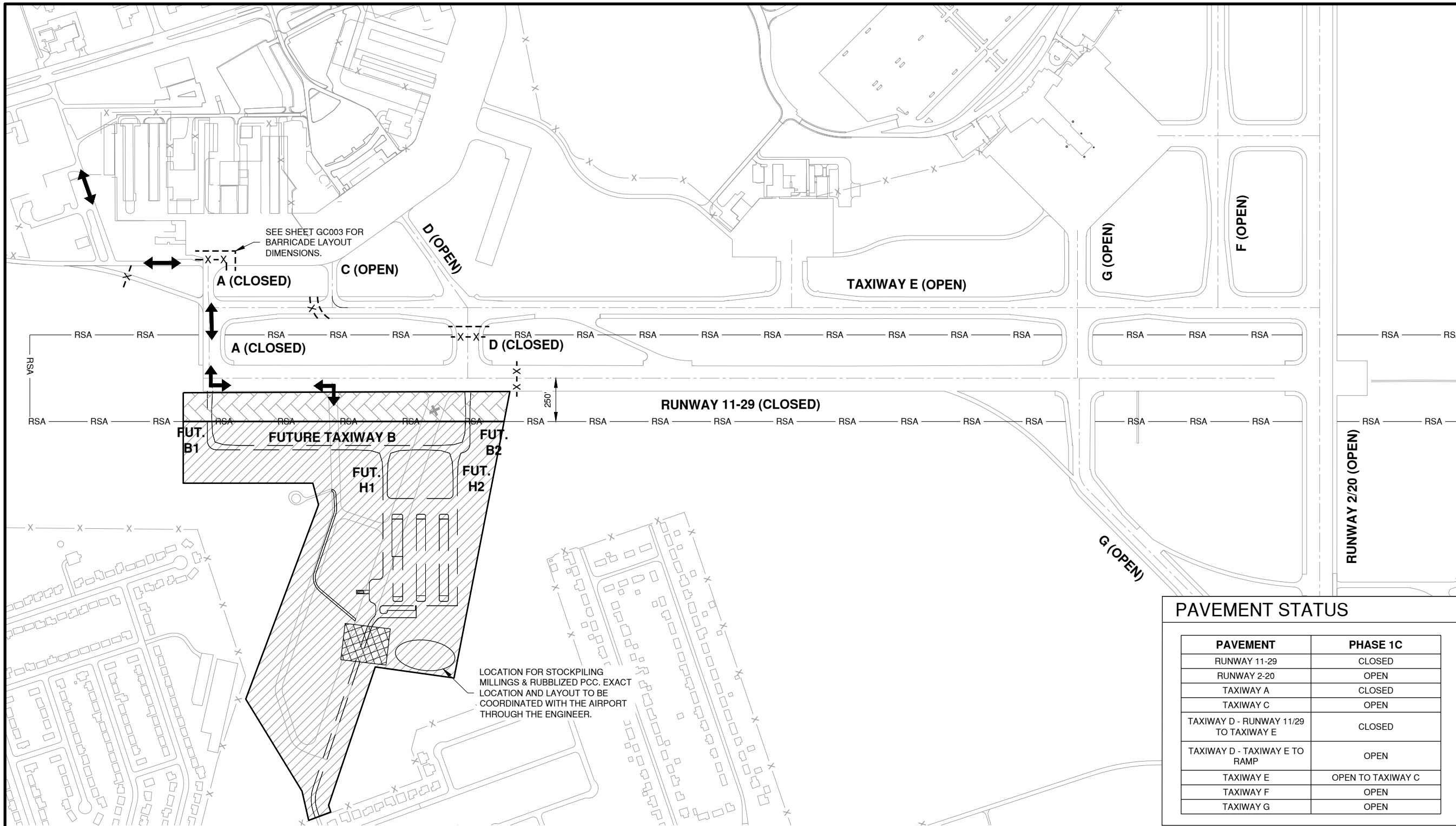


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SEE SHEET GC003 FOR BARRICADE LAYOUT DIMENSIONS.

A (CLOSED)

C (OPEN)

D (OPEN)

TAXIWAY E (OPEN)

G (OPEN)

F (OPEN)

A (CLOSED)

D (CLOSED)

RUNWAY 11-29 (CLOSED)

FUT. B1  
FUTURE TAXIWAY B

FUT. H1

FUT. H2

LOCATION FOR STOCKPILING MILLINGS & RUBBLIZED PCC. EXACT LOCATION AND LAYOUT TO BE COORDINATED WITH THE AIRPORT THROUGH THE ENGINEER.

RUNWAY 2/20 (OPEN)

PAVEMENT STATUS

PAVEMENT	PHASE 1C
RUNWAY 11-29	CLOSED
RUNWAY 2-20	OPEN
TAXIWAY A	CLOSED
TAXIWAY C	OPEN
TAXIWAY D - RUNWAY 11/29 TO TAXIWAY E	CLOSED
TAXIWAY D - TAXIWAY E TO RAMP	OPEN
TAXIWAY E	OPEN TO TAXIWAY C
TAXIWAY F	OPEN
TAXIWAY G	OPEN

100% FOR BIDDING  
MAY 23, 2022

CONSTRUCT NEW GENERAL AVIATION FACILITY - PHASE I

OWNER



BLOOMINGTON-NORMAL AIRPORT AUTHORITY  
CENTRAL ILLINOIS REGIONAL AIRPORT  
BLOOMINGTON, ILLINOIS

6/21/22 ADDENDUM #2

MARK DATE DESCRIPTION

AIP PROJ. NO. 3-17-0006-071/077

IL PROJ. NO. BMI-4539

CMT PROJECT NO: 19008501-10

CAD DWG FILE: 19008501-PH1-GC100.DWG

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**CONSTRUCTION ACTIVITY PLAN PHASE 1-C**

GC103

SHEET 12 OF 88

LEGEND

- AVAILABLE WORK AREA FOR THIS PHASE
- CONTRACTOR STAGING & STORAGE AREA
- WORK AREA FOR THIS PHASE TO BE RE-OPENED AT THE END OF EACH WORK DAY
- X-X-X-X- IDOT TYPE I BARRICADES
- BEAM BARRICADES
- CONTRACTOR ACCESS ROUTE

PHASE 1-C NOTES

1. RUNWAY 11/29 WILL BE CLOSED FOR THIS PHASE.
2. ALL EQUIPMENT SHALL BE MOVED TO THE STAGING AREA AT THE END OF EACH WORK PERIOD.
3. THE CONTRACTOR SHALL PROVIDE 72 HOURS NOTICE PRIOR TO INITIATING THIS PHASE.
4. THE CONTRACTOR SHALL PLACE ALL BARRICADES PRIOR TO STARTING WORK ON THIS PHASE.
5. THIS PHASE MAY BE CONCURRENT WITH PHASE 1-A.
6. DURING THIS PHASE, THE WORK WITHIN 250' OF RUNWAY 11/29 CENTERLINE SHALL RECEIVE PRIORITY OVER ALL OTHER WORK AREAS.
7. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN THE ACCESS ROUTE AND AT THE END OF THE WORKDAY.
8. AIRCRAFT AND AIRPORT VEHICLES HAVE THE RIGHT OF WAY AT ALL TIMES.
9. RUNWAY 11/29 SHALL BE CLOSED AFTER 7:00 AM AND REOPENED BY 7:00 PM ON A DAILY BASIS.
10. THE CONTRACTOR WILL BE ALLOWED TO CLOSE RUNWAY 11/29 FOR NO MORE THAN 10 NON-CONSECUTIVE DAYS TO PERFORM THE WORK WITHIN THIS PHASE.
11. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT TO SCHEDULE THE CLOSURE. VARIABLES SUCH AS THE WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT AVAILABILITY & CONDITION, AND WORKFORCE AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE.
12. PRIOR TO RE-OPENING RUNWAY 11/29, THE CONTRACTOR SHALL MEET THE FOLLOWING REQUIREMENTS WITHIN 250' OF RUNWAY 11/29 CENTERLINE:
  - 12.1. CLEARED AND GRADED AND HAVE NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS.
  - 12.2. DRAINED BY GRADING OR STORM SEWERS TO PREVENT WATER ACCUMULATION.
  - 12.3. CAPABLE, UNDER DRY CONDITIONS, OF SUPPORTING AIRPORT MAINTENANCE EQUIPMENT, AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) EQUIPMENT, AND THE OCCASIONAL PASSAGE OF AIRCRAFT WITHOUT CAUSING DAMAGE TO THE AIRCRAFT.
  - 12.4. FREE OF OBJECTS HIGHER THAN 3" ABOVE GRADE INCLUDING SOIL MOUNDS, SURFACE IRREGULARITIES, OR CONSTRUCTION MATERIALS.
  - 12.5. OPEN TRENCHES OR EXCAVATIONS MUST BE BACKFILLED.
13. FOR THIS PHASE, THE CONTRACTOR SHALL PROVIDE A FULL TIME ESCORT IN CONSTANT RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) TO ESCORT HIS EMPLOYEES, SUBCONTRACTORS, AND DELIVERIES TO AND FROM THE AVAILABLE WORK AREA. ALL ACCESS TO AND FROM THE AVAILABLE WORK AREAS SHALL BE UNDER THE CONTROL OF THE CONTRACTOR'S ESCORT.
14. INGRESS AND EGRESS TO THE PROPOSED WORK AREAS WILL REQUIRE ACCESS TO AND CROSSING OF PAVEMENTS UNDER ACTIVE AIR TRAFFIC CONTROL TOWER (ATCT) CONTROL. TO MINIMIZE IMPACTS TO AIR TRAFFIC AND THE ATCT WORKLOAD, THE CONTRACTOR SHALL COORDINATE AND PLAN THEIR ACCESS TO AND FROM THE SITE TO OCCUR WITHIN THE MINIMUM NUMBER OF TRIPS AS POSSIBLE.
15. THIS PHASE WILL RELY HEAVILY ON COORDINATION WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) THROUGH RADIO COMMUNICATIONS TO COMPLETE THIS WORK. THE CONTRACTOR WILL NOT BE ABLE TO ACCESS THIS WORK AREA WITHOUT BEING UNDER THE NECESSARY SECURITY AND RADIO CONTROL.

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